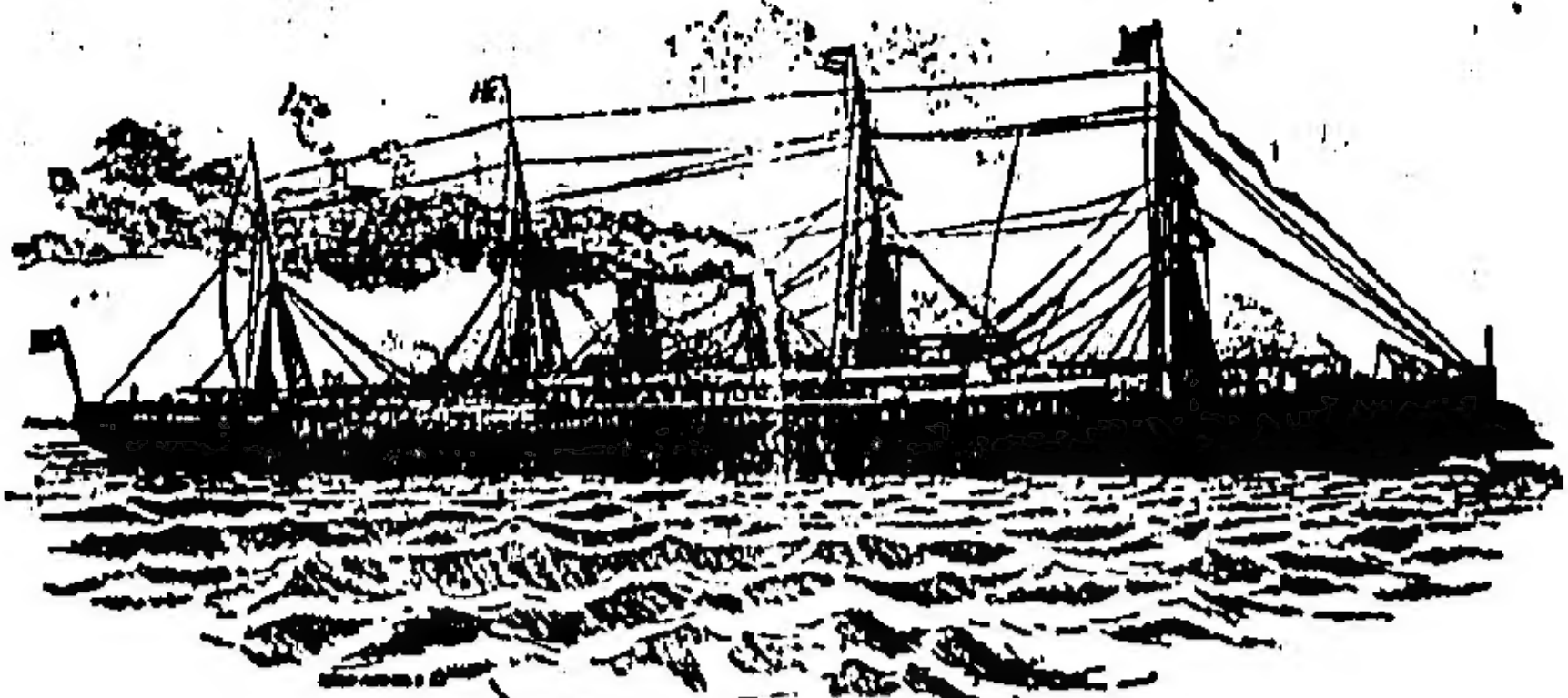






## Mails.

## U.S. MAIL LINES.



PACIFIC MAIL S.S. CO., OCCIDENTAL & ORIENTAL S.S. CO.,  
TOYO KISEN KAISHA.

TAKING PASSENGERS AND CARGO TO JAPAN, THE UNITED STATES, MEXICO,  
CENTRAL AND SOUTH AMERICA AND EUROPE.

## PROPOSED SAILINGS FROM HONGKONG.

"CHINA"	5,060 Gross Tons	TUESDAY, 5th April, at Noon.
"DORIO"	4,784 "	SATURDAY, 16th April, at Noon.
"SIBERIA"	11,284 "	SATURDAY, 30th April, at Noon.
"GOPTIC"	4,352 "	THURSDAY, 12th May, at Noon.
"KOREA"	11,276 "	TUESDAY, 24th May, at Noon.
"GAELIC"	4,205 "	SATURDAY, 4th June, at Noon.
"MONGOLIA"	"	THURSDAY, 16th June, at Noon.

Record Trip Yokohama to San Francisco made by s.s. "KOREA," 11,276 tons, Oct. 18th-28th, 1902; 10 days, 15 hours.

THE P. M. Company's Steamship "CHINA" will be despatched for SAN FRANCISCO, via SHANGHAI, NAGASAKI, KOBE, INLAND SEA, YOKOHAMA and HONOLULU, on TUESDAY, the 5th April, at Noon, taking Freight for Japan, the United States, and Europe. Passengers are allowed to break their journey at any point on route.

Through Passage Tickets granted to England, France and Germany by all trans-Atlantic lines of Steamers, and to the principal cities of the United States or Canada.

Passengers holding through ORDERS TO EUROPE have the choice of the Overland Rail Routes from San Francisco, including the SOUTHERN PACIFIC, CENTRAL PACIFIC, UNION PACIFIC, DENVER and RIO GRANDE, and NORTHERN PACIFIC RAILWAY; also the CANADIAN PACIFIC RAILWAY on payment of £4 in addition to the regular tariff rate.

Passengers holding Orders for OVERLAND CITIES in the United States have between SAN FRANCISCO and CHICAGO, the option of the SOUTHERN PACIFIC, CENTRAL PACIFIC, UNION PACIFIC, DENVER and RIO GRANDE, and other direct connecting Railways, and from Chicago to destination the choice of direct lines.

Special rates (First-class only) to European Ports, are granted to Missionaries, Members of the Naval, Military, Diplomatic and Consular Services, and European Civil Service Officials located in Asia, and to European Officials in the Service of the Governments of China and Japan.

TO UNITED STATES AND CANADIAN PORTS, Special rates (first class only) are granted and will apply only to Missionaries, Members of the Naval and Military Services, and to Consular and Diplomatic Officials of the Governments of China and Japan.

Through Bills of Lading issued for transportation to Yokohama and other Japan Ports, to San Francisco, to Atlantic and Inland Cities of the United States, via Overland Railway, to Havana, Trinidad, and Demerara, and to ports in Mexico, Central and South America, by the Companies' and connecting Steamers.

## FEATURES OF THIS LINE.

The largest and steadiest and fastest passenger ships on the Pacific.

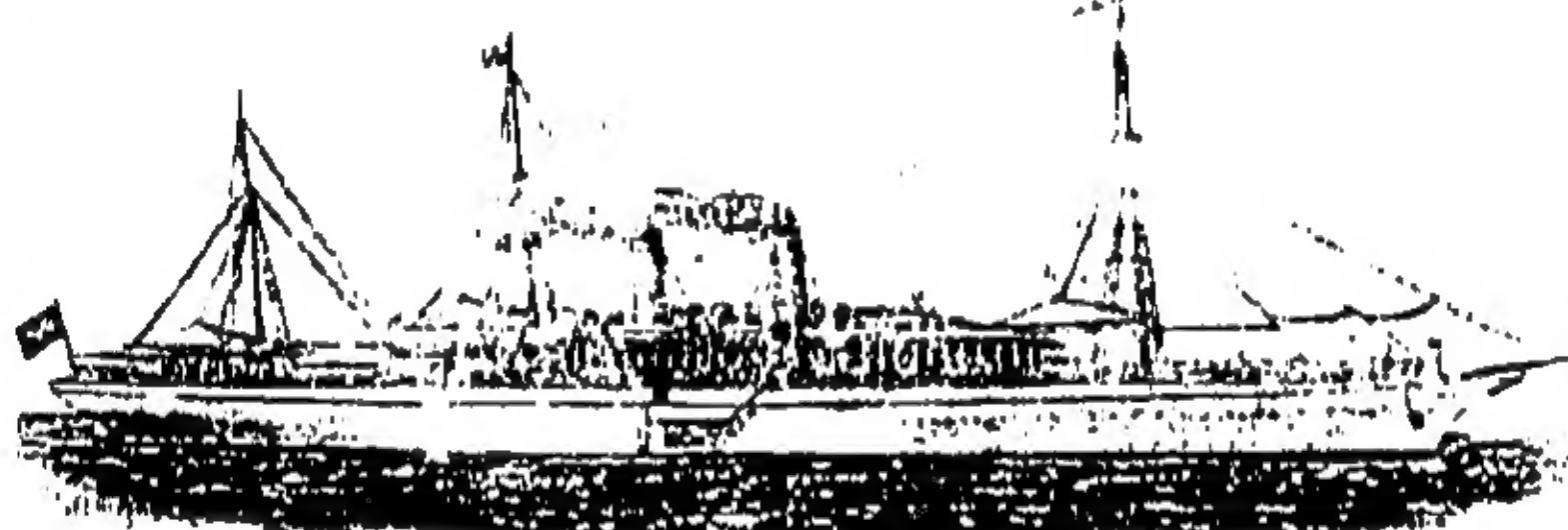
Southern Route; passengers enjoy out-of-doors throughout; deck bathing. The call at Honolulu, Oahu, the most fertile and beautiful island of the Pacific. The only line to San Francisco, the greatest port of the Pacific.

Sailings positively on schedule date.

For further information as to Passage and Freight, apply to the Agency of the Companies, Queen's Building.

E. W. TILDEN, Agent.

Hongkong, 2nd April, 1904.

CANADIAN PACIFIC RAILWAY COY.'S  
ROYAL MAIL STEAMSHIP LINE.

THE FAST ROUTE BETWEEN CHINA, JAPAN AND EUROPE, VIA CANADA  
AND THE UNITED STATES.  
(CALLING AT SHANGHAI, NAGASAKI, KOBE, YOKOHAMA & VICTORIA, B.C.)  
SAFETY. SPEED. PUNCTUALITY.

SAVING 3 TO 7 DAYS ACROSS THE PACIFIC.

"EMPRESS" Twin Screw Steamships—6,000 Tons—10,000 Horse Power—Speed 19 Knots.

PROPOSED SAILINGS FROM HONGKONG.—(SUBJECT TO ALTERATION).

R.M.S. "EMPRESS OF INDIA" ... 6,000 Tons ... WEDNESDAY, 20th April.

"ATHENIAN" ... 3,882 " ... WEDNESDAY, 27th April.

"EMPRESS OF JAPAN" ... 6,000 " ... WEDNESDAY, 11th May.

"TARTAR" ... 4,425 " ... SATURDAY, 21st May.

"EMPRESS OF CHINA" ... 6,000 " ...

Hongkong to London, 1st Class ... via St. Lawrence £60. Via New York £62.

Hongkong to London, Intermediate ... £40.

Steamers, and 1st Class Rail ... £42.

THE magnificent Twin-screw "EMPRESS" Steamships pass through the famous INLAND SEA OF JAPAN, and usually make the voyage YOKOHAMA TO VANCOUVER (B.C.) in 12 DAYS, and make connection with the PALATIAL OVERLAND TRAINS FROM THE PACIFIC TO THE ATLANTIC WITHOUT CHANGE.

Passengers Booked through to all principal points and AROUND THE WORLD.

SPECIAL RATES (First class only) granted to Missionaries, Members of the Naval, Military, Diplomatic and Civil Services, and to European Officials in the Service of Chinese and Japanese Governments.

For further information, Maps, Guides, Hand Books, Rates of Freight and Passage, apply to

Hongkong, 30th March, 1904.

J. E. BROWN, General Agent,

9, Pedder's Street.

[10]

HAMBURG-AMERIKA LINIE.  
OSTASIATISCHER FRACHTDAMPFER DIENST.

(Taking Cargo at through Rates to ANTWERP, AMSTERDAM, ROTTERDAM, COPENHAGEN, LISBON, OPORTO, LONDON, LIVERPOOL, GLASGOW, TRIESTE, GENOA, PORTS IN THE LEVANT; BLACK SEA and BALTIC PORTS; NORTH and SOUTH AMERICAN PORTS).

## PROPOSED SAILINGS FROM HONGKONG.

## SUBJECT TO ALTERATION.

STEAMERS.	DESTINATIONS.	SAILING DATES.
ABESSINIA	ST. NAZAIRE, HAVRE & HAMBURG.	2nd April.
Filler	(Calling at SINGAPORE and COLOMBO).	Freight.
BRISGAVIA	HAMBURG.	12th April.
Schiller	(Calling at SINGAPORE and COLOMBO).	Freight.
SUEVIA	HAVRE and HAMBURG.	25th April.
von Döhren	(Calling at SINGAPORE and PENANG).	Freight.
ARTEMISIA	HAVRE and HAMBURG.	3rd May.
Grossmeyer	(Calling at SINGAPORE and COLOMBO).	Freight.
MARBURG	HAVRE and HAMBURG.	17th May.
Stem	(Calling at SINGAPORE and PENANG).	Freight.
STRASSBURG	HAVRE and HAMBURG.	31st May.
Madsen	(Calling at SINGAPORE and COLOMBO).	Freight and Passengers.

For further Particulars, apply to

HAMBURG-AMERIKA LINIE,

HONGKONG OFFICE,

No. 1, Queen's Buildings.

Hongkong, 16th March, 1904.

[16]

GO TO THE  
KOWLOON HOTEL,  
KOWLOON.

J. W. OSBORNE,  
Proprietor and Manager.

## Shipping—Steamers.

HONGKONG, CANTON, MACAO AND  
WEST RIVER STEAMERS.

JOINT SERVICE OF THE HONGKONG, CANTON AND MACAO  
STEAMBOAT CO., LTD. AND THE CHINA NAVIGATION  
COMPANY, LTD.

## HONGKONG-CANTON LINE.

S.S. "HONAM"	2,363 tons	Captain R. D. Thomas.
"POWAN"	2,338 "	G. F. Morrison, R.N.R.
"FATSHAY"	2,260 "	W. A. Valentine.
"HANKOW"	3,073 "	C. V. Lloyd.
"KINSHAW"	2,800 "	J. J. Lossius.

Departures from HONGKONG to CANTON daily at 8 A.M. (Sunday excepted) and at 5:30 P.M. and 9 P.M. (Saturday excepted).

Departures from CANTON to HONGKONG daily at 8 A.M., 2:30 P.M. and 5:30 P.M. (Sunday excepted).

These Steamers, carrying His Majesty's Mails, are the largest and fastest on the River

Special attention is drawn to their Superior Saloon and Cabin accommodation.

SERVICE OF THE HONGKONG, CANTON AND MACAO STEAMBOAT CO., LTD.

## HONGKONG-MACAO LINE.

S.S. "HEUNGSHAN"	1,998 tons	Captain H. D. Jones.
------------------	------------	----------------------

Departures from Hongkong to Macao on week days at about 2 P.M.

During the summer months the time of leaving fluctuates to suit the tide at Macao. For further particulars see special time table.

Departures on Sundays at 12:30 P.M.

Departures from Macao to Hongkong daily at 7:30 A.M.

## CANTON-MACAO LINE.

S.S. "LUNGSHAN"	2,119 tons	Captain T. Hamlin.
-----------------	------------	--------------------

This steamer leaves Canton for Macao every Tuesday, Thursday and Saturday at about 7:30 A.M.; and leaves Macao for Canton every Monday, Wednesday and Friday at about 7:30 A.M.

JOINT SERVICE OF THE H.K., C. AND MACAO STEAMBOAT CO., LTD., THE CHINA NAVIGATION COMPANY, LTD., AND THE INDO-CHINA STEAM NAVIGATION COMPANY, LTD.

## CANTON-WUCHOW LINE.

S.S. "SAINAM"	1,588 tons	Captain B. Branch.
"NANNING"	1,559 "	C. Butchart.

One of the above Steamers leaves Canton for Wuchow every Monday, Wednesday and Friday at about 8 A.M., and the other leaves Wuchow for Canton on the same days at about 8 A.M. Round trips take about 5 days. These vessels have Superior Cabin Accommodation and are lighted throughout by electricity.

Further particulars may be obtained at the Office of the—

HONGKONG, CANTON & MACAO STEAMBOAT CO., LD.,

18, Bank Buildings, Queen's Road Central, opposite the Hongkong Hotel

Or of BUTTERFIELD & SWIRE,

Agents, CHINA NAVIGATION CO., LTD.

Hongkong, 2nd April, 1904.

## Intimations.

PORTRAITS, GROUPS, ENLARGING, AND  
COPYING IN ALL SIZES.AMATEUR WORK GIVEN SPECIAL  
ATTENTION.FULL LINE OF SUPPLIES  
ALWAYS IN STOCK.ORIENTAL  
COSTUMES AND  
FANCY DRAPERIES  
FURNISHED.WORK GUARANTEED TO BE  
THE BEST IN THE COLONY.

## LADIES' SPECIAL TOILET ROOM.

## PATRONAGE RESPECTFULLY SOLICITED.

THE CONNAUGHT HOUSE,  
QUEEN'S ROAD CENTRAL.

A FIRST CLASS HOTEL SITUATED NEAR THE BANKS AND PRINCIPAL  
OFFICES. EXCELLENT CUISINE AND WINES.

Large and lofty Rooms Elegantly Furnished. Hydraulic Elevator.

Hot and Cold Water throughout. Special Rates for Tourists.

Launch Service for Guests.

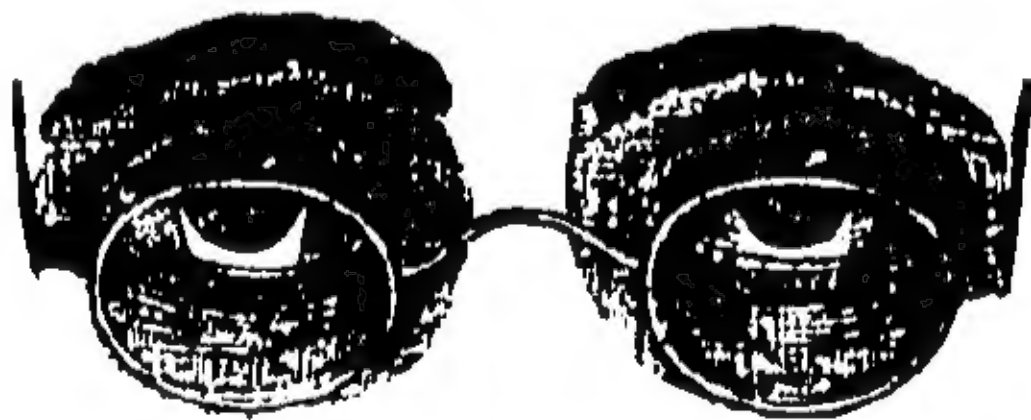
For Terms apply to

Hongkong, 1st November, 1903.

THE MANAGER.

[29]

## EYE-SIGHT.



## MR. N. LAZARUS

May be personally consulted for SPECTACLES.

No charge for testing the eyes.

Glasses and frames of all kinds and qualities.

Prices from \$2 upwards.

16, QUEEN'S ROAD, CENTRAL.

Hongkong, 6th November, 1903.

[44]

## F. BLACKHEAD &amp; CO.,

SHIP-CHANDLERS, SAILMAKERS,  
COAL AND PROVISION MERCHANTS,  
NAVAL CONTRACTOR,  
AND GENERAL COMMISSION  
AGENTS.

16, DES VŒUX ROAD CENTRAL,  
HONGKONG.

SOAP AND SODA MANUFACTURERS.

SOLE AGENTS FOR

HARTMANN'S RAHTJEN'S GENUINE

COMPOSITION RED HAND

BRAND, HARTMANN'S GREY PAINT,

DAIMLER'S PATENT MOTOR

LAUNCHES,

&c., &c., &c.

Sole Agents for

FERGUSON'S SPECIAL CREAM

and

P. & O. SPECIAL LIQUOR SCOTCH

WHISKY, &c.

EVERY KIND OF

SHIPS STORES AND REQUISITES

ALWAYS IN STOCK

AT

REASONABLE PRICES.

Hongkong, 15th December, 1903.

[18]

## THE HONGKONG

## STUDIO,

HIGHER CLASS PHOTOGRAPHER,  
41 & 43, QUEEN'S ROAD CENTRAL,  
TOP FLOOR.

PORTRAITS, GROUPS and ENLARGING  
and COPYING in all sizes.

LARGE SELECTION OF VIEWS ALWAYS  
ON HAND.

PRICE VERY MODERATE

Hongkong, 15th September, 1903.

[1]

## MEE CHEUNG,

## PHOTOGRAPHER,

TOP FLOOR OF ICE HOUSE, IN  
ICE-HOUSE ROAD.

Now in a position, in his New and Com-

modious Premises, to eclipse, as heretofore,

ALL PHOTOGRAPHIC ART PRACTICED

in the Colony or in any part of the Far East.

GROUPS and VIEWS

speciality

Hongkong, 22nd September, 1903.

[15]

## Intimations.

## THE YOKOHAMA DOCK CO., LTD.

## No. 1 DOCK.

Length inside, 514 ft. Width of  
entrance, top 95 ft.; bottom 75 ft.  
Water on blocks, 27.5 ft. Time to  
pump out, 4 hours.

## No. 2 DOCK.

Length inside, 375 ft. Width of  
entrance, top 80.5 ft.; bottom 45.8  
ft. Water on blocks, 28.5 ft. Time  
to pump out, 2 hours.

THESE DOCKS are conveniently situated in Yokohama harbour and the attention of  
Captains and Engineers is respectfully called to the advantages offered for Dock-  
ing and repairing Vessels and Machinery of every description.

The plant and tools are of recent patterns for dealing quickly and cheaply with work,  
and a large stock of material is always at hand, (plates and angles all being tested by  
Lloyds' surveyors).

Two powerful Twin Screw Towboats are available for taking Vessels in or out of  
Dock, and for taking Sailing Vessels in or out of the bay. The floating derrick is capable  
of lifting 35 tons.

Steam Launches of Steel or Wood, Lighters, Steel Buildings and Roofs, Bridge  
Work, and all kinds of Machinery are made on the premises.

Tenders will be made up when required and the workmanship and material will be  
guaranteed.

The cost of Docking, and repair work, will be found to compare favourably with that  
of any port in the world.

Telephone: Works, No. 508; General, No. 378.

Telegrams, "Dock, Yokohama," Codes A. I. and A. B. C. (4th).

Yokohama, May 11th, 1903.

[G]

## HOTEL CRAIGIEBURN,

PLUNKET'S GAP, the PEAK, near the TRAM TERMINUS, Tel. 56.

For Terms, &c., apply to the

MANAGER.

Hongkong, 2nd July, 1900.

[32]

CHINESE IMPERIAL GOVERNMENT  
7 PER CENT SILVER LOAN  
OF 1886, E.

## 35TH HALF-YEARLY DRAWING.

INTEREST DUE AND DRAWN BONDS OF  
this LOAN will be PAYABLE at the  
OFFICES of the CORPORATION on or after the  
31st March, 1904.

LIST OF DRAWN BONDS can be obtained  
on application to the Undersigned.

For the HONGKONG AND SHANGHAI  
BANKING CORPORATION,

Agents issuing the Loan,

J. R. M. SMITH, Chief Manager.

Hongkong, 30th March, 1904.

[451]

## WANTED.

SHORTHAND CLERKS and TYPE-  
WRITERS, must know the "UP-TO-  
DATE" system.

Address:—

"R,"

C/o The Hongkong Telegraph.

Hongkong, 28th March, 1904.

[441]

## POSITION WANTED.

YOUNG ENGLISHMAN,  
at present in Japan,

is desirous of obtaining a position in a Mercan-

tile Firm in Hongkong.

Advertiser has had considerable experience in

SHIPPING, GENERAL OFFICE WORK,

AND

COMMERCIAL CORRESPONDENCE.

Can furnish good references.

Apply, in first instance, to

"G,"

Care this paper.

Hongkong, 28th March, 1904.

[438]

WINDSOR GARDEN  
AND RESTAURANT,

WONG-NEI-CHONG ROAD,  
(HAPPY VALLEY),  
Past the Race-Course.

UNDER EUROPEAN MANAGEMENT.

MEALS AND REFRESHMENTS

are served in style at reasonable prices,  
and the

Attendance will be found satisfactory.

PARTIES' DINNERS, PICNICS, &c.,  
can always be arranged for through the

Manager.

The Scenery of and the View from the

Garden would be hard to beat, and many

customers have called it the loveliest spot in







## Intimations.

WATSON'S  
TOILET PREPARATIONS

**WATSON'S GLYCERINE AND CARBOLIC SOAP'S** effect a saving of 50% owing to the large size of the tablets. They are made of the purest ingredients and are elegantly put up. Our Carbolic Dog Soap is the best thing of its kind in the market.

**WATSON'S TAI YUEK FONG HAIR WASH** prepared from a recipe of the late Dr. Ayres, continues to give much satisfaction to those who use it.

**WATSON'S ORIENTAL DENTIFRICE.** In the early days of the Colony the public used no other. Liquid dentifrices do not keep the teeth white and clean. We recommend the above preparation to all, and especially to those who are heavy smokers.

**A. S. WATSON & CO., LIMITED.**

THE HONGKONG DISPENSARY.

MANUFACTURING CHEMISTS.

ESTABLISHED 1841.

Hongkong, 26th March, 1904.

TELEPHONE NO. 10.  
CABLE ADDRESS: "ACHEE," HONGKONG  
A. B. C. CODE, 4TH EDITION

ESTABLISHED 1859

**A CHEE & CO.,**  
祥利廣  
17, QUEEN'S ROAD.

**FURNITURE DEALERS.**

DRAWING-ROOM,

DINING-ROOM,

and BED-ROOM

FURNITURE.

ELECTRO-PLATED,

GLASS, and

CHINA WARES.

PASTEUR'S MICROBE-PROOF

FILTERS,

ROCHESTER LAMPS,

WHITE TURKISH TOWELS.

COUNTERPANES.

COOKING RANGES,

KITCHEN UTENSILS, and

HOUSEHOLD REQUISITES.

**PHOTOGRAPHIC DEPARTMENT.**

DEVELOPING and PRINTING

UNDERTAKEN FOR AMATEURS.

GOOD WORK.

PROMPT RETURN.

Hongkong, 8th January, 1904.

**CYPRIDOL CAPSULES.**

THE MODERN REMEDY for CON-

TAGIOUS SKIN DISEASES does

not salivate or affect the gums like Mercury.

Doctors recommend them.

BOTTLES of 50 Capsules...\$2.75 each

TO BE HAD AT

**THE PHARMACY**

14, QUEEN'S ROAD CENTRAL, 法中

Hongkong.

A. STEVENSON,

Chemist,

Registered by the Pharmaceutical Society.

Hongkong, 2nd April, 1904.

All communications intended for publication in the "HONGKONG TELEGRAPH" should be addressed to The Editor, 1, Ice House Road, and should be accompanied by the Writer's Name and Address.

Ordinary business communications should be addressed to The Manager.

The Editor will not undertake to be responsible for any rejected MS., nor to return any Contribution.

**SUBSCRIPTION RATES (IN ADVANCE).**

DAILY—\$10 per annum.

WEEKLY—\$13 per annum.

The rates per quarter and per annum, proportional.

The daily issue is delivered free when the address is accessible to messenger. On copies sent by post an additional \$1.80 per quarter is charged for postage.

The postage on the weekly issue to any part of the world is 30 cents per quarter.

Single Copies Daily, Ten cents; Weekly, twenty-five cents.

## The Hongkong Telegraph

HONGKONG, SATURDAY, APRIL 2, 1904.

## PRAYA EAST RECLAMATION.

It will be matter for satisfaction to the Colony generally, and to Marine Lighthouse on the Praya East in particular, to know that the negotiations relating to the reclamation of the eastern foreshore of the Island, initiated by the Honourable Sir Paul Chater, C.M.G., and acquiesced in by the property owners, have been brought to a satisfactory issue with the exception of those relating to the boundary between the proposed scheme and the Naval and Military properties. These were still in progress at the close of last year, having proved to be very protracted and troublesome. It is evident that the Public Works authorities believe in taking time by the forelock. The head of that department in his last annual report, printed in the current number of the *Gazette*, states that, in consequence of the proposed construction of the electric tramway along Praya East, it was considered advisable to proceed at once with the raising of that roadway to such a level as would admit of the drains and sewers being extended hereafter across the reclamation to the New Praya front, thus obviating the taking up and re-laying of the tramway at an early date. A contract for the work was let in November, a premium being offered for its speedy completion. Very satisfactory progress was made, and by the close of the year the raising was practically completed from Arsenal Street to No. 2 Police Station, whilst a considerable amount had been done on the remaining portion of the road to the eastward. The space to be occupied by the tramway was left about a foot below the finished surface of the road so as to save the filling in and subsequent digging out of material over this area. The first commencement of the actual work of reclamation is looked for with eagerness, since the completion of the scheme will render available a large area of much-needed building land, centrally situated, which is destined to provide accommodation for the working classes of the Colony and so help to relieve the acute congestion of the population now so densely huddled together in certain districts of Hongkong.

## LOCAL AND GENERAL.

GENERAL Ventris accidentally fractured his leg on the 12th ult. He is reported to be progressing favourably.

VERY extensive quarrying operations are now carried on, in the New Territory, notably at Chai Kwo Leng, Yee-nun and Ngau-Tau Kok.

The Governor of Foochow has sent a quantity of rifles, ammunition and some T.M. 9,000 to the Viceroy of Liang Kwang as a contribution to help him in restoring order in these provinces.

His Majesty the King has not been advised to exercise his power of disallowance with respect to the Ordinance to amend the Public Health and Buildings Ordinance, 1903, (No. 1 of 1903).

Mr. J. F. Boulton has been appointed to act as Assistant Director of Public Works on and from the 30th ultimo for so long as Mr. P. N. H. Jones is acting as Director of Public Works, or until further notice.

AN area of about four acres of land, mostly hillside, on the promontory west of Lai Chi Kok, was placed by the Colonial Government disposal of the Naval authorities in connection with the establishment of a torpedo range in that neighbourhood.

A GENERAL map of the New Territory on a scale of two inches to the mile is now being prepared. It will show all streams, roads, paths, cultivated areas and villages, the names of the latter being marked, whilst the hills will be indicated by contours.

THE number of squatters' licences on the roll at the commencement of last year was 980, representing an annual rent of \$1,429 63.

THE Hon. Treasurer of the Alice Memorial and Nethercole Hospitals begs to acknowledge with thanks the following donations to the funds of the Hospitals:—

Taikoo Sports... \$52.80

Ko Cheuk Shing... 25

H. E. the Officer Administering the Government has been pleased to provisionally appoint Mr. P. N. H. Jones, Acting Director of Public Works, to be an official member of the Executive and Legislative Councils, during the absence of Mr. W. Chatham or until further notice.

THE Board of Revenue in Peking has had the brilliant idea of collecting the taxes partly in silver and partly in gold, under the impression that they will thereby obtain a supply of the yellow metal and be able to institute a gold currency. The Throne is said to have approved this plan of financial genius.

THE firm of Atchaf and Armstrong have received an order from Turkey for seven torpedo boats to cost £30,000. This is the same firm which supplied the two cruisers *Merak* and *Kasuga* for Japan, which passed through Colombo last month. It looks very much as if Turkey is making preparations for the war for which every Turk is longing.

THE Imperial Chinese Post Office in Liu Kung Tau and Port Edward, have been withdrawn from yesterday and all postal business in the dependency of Weihaiwei will be conducted through the British Agency at Liu Kung Tau. The Imperial Chinese Post Office in the city of Weihaiwei will be retained for the transmission of Chinese correspondence to the interior. The British Agency at Liu Kung Tau will open a branch office at Port Edward.

By kind permission of Major Riddell and officers of the Band of the 93rd Burma Infantry will play at the Hongkong Hotel this evening from 8 to 9.30 p.m.

**BAND PROGRAMME.**

March: "The Stars and Stripes for Ever," Sousa.

Introduction: "The Stars and Stripes for Ever," Sousa.

Selection: "The Stars and Stripes for Ever," Sousa.

March: "The Stars and Stripes for Ever," Sousa.

Selection: "The Stars and Stripes for Ever," Sousa.

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Selection: "The Stars and Stripes for Ever," Sousa.

March: "The Stars and Stripes for Ever," Sousa.

Selection: "The Stars and Stripes for Ever," Sousa.

A MAIL paper has the following interesting points about the Siberian railway: Number of stations on complete line, 39; rail on railway, 14,738; number of locomotives, 751; number of passenger carriages, 5,8; number of goods waggons, 7,743; number of mail vans, 33; miles of line in Siberia, 3,539; miles of line in Manchuria, 1,444; Moscow to Irkutsk, 8 days; Irkutsk to Manchuria (7.1 miles rail and 5.1 miles ferry), 74 hours; across Manchuria, 949 miles to Port Arthur, 77 hours; former east terminus for Vladivostok, Strelnik; west terminus of Vladivostok railway Khabarovsk; river steamer route to connect Khabarovsk and Strelnik, 1,340 miles; Vladivostok to London by rail (7,092 miles); 174 days.

## THE JUBILEE ROAD.

Mr. W. Chatham thus describes the Jubilee Road in his annual report just published.

The road commences at Kennedy Town and traverses the southern slopes of Mount Davis and the adjoining hills to Aberdeen, opening out *en route* a portion of the island which has hitherto been practically inaccessible. It is 5 miles long has a width of 16 feet and is designed as a carriage road.

Leaving Kennedy Town, it rises for about a mile with gradients of 1 in 20 and 1 in 30 until it attains a height of about 180 feet above sea level; one of the conditions upon which the Military Authorities withdrew their opposition to its construction being that it should be kept 150 feet above the sea. Passing through a small gap adjoining a site acquired by the Military Authorities for gun practice, it falls at 1 in 30 until it reaches the 150 feet contour, when it is carried level round Sandy Bay to below Bisnet Villa. Another ascent at 1 in 30 is then made to a gap about 250 feet above sea level near Sassoon's Villa, at which point there is a cross road connecting it with the old road from West Point to Aberdeen, which is at a considerably greater altitude. From the gap, it again falls at the same gradient to the 150 feet contour and then runs level through the property of the Missions Etrangères. Crossing the Pokfulam Stream on a bridge of 4 arches, a little further on, it joins the old road to Aberdeen, of which a length of 800 feet has been utilized after improving the gradients. Diverging again at No. 10 Bridge, which has been widened considerably, it falls at 1 in 20 and finally terminates in close proximity to the Aberdeen Docks by joining the old road near sea level, the reason for constructing this portion being the excessively steep gradients of the latter before the bridge mentioned. The road is surfaced throughout with decomposed granite.

There are 10 bridges, all of which are built of granite masonry, with brick arches faced with granite, the spans varying from 10 to 17 feet. All bridges and retaining walls are provided with parapet walls of granite masonry, whilst other parts of the road are protected by iron railings where necessary. The pathway before referred to commences near the bridge across the main stream on the road from the Peak to Aberdeen and joins the Jubilee Road near the No. 10 Bridge already mentioned.

The work was carried out for the Committee by Messrs. Denison, Ram and Gibbs, Engineers, Mr. Li A. Ping being the contractor. The total expenditure, including all charges, was \$15,274.27, the amount expended under the contract being \$106,146. These figures include the sum of \$5,245, which was paid by Government.

## BREAKDOWN OF THE N.D.L.

"OLDENBURG."

## DEPARTURE DELAYED.

We have received the following communication, dated 1st instant, from Messrs. Melchers & Co., local agents of the N.D.L.:—"The agent of the Norddeutscher Lloyd at Aden wired on the 30th March:—'*Oldenburg* cannot sail on account of shaft broken probably departure 18th April' and we would therefore kindly ask you to publish that the departure for Europe of the Imperial German Mail s.s. *Oldenburg* will be delayed for about three weeks."

## MILITARY LANDS.

Negotiations between the Colonial Government and the Military Authorities, for an exchange of land are still proceeding. Pending a settlement, the D. P. W. reports, the Military Authorities were allowed into possession of considerable areas at Pinewood Battery, below High West Gap, and at a point on the Jubilee Road, about a mile beyond the Plague Hospital. Arrangements were also made for the construction of new Rifle Ranges behind Kowloon City under the supervision of the Royal Engineers and at the expense of the Colony. This is necessary to enable the present ranges to be relinquished with a view to the formation of the King's Park in Kowloon.

## BUILDING ACTIVITY

## IN THE COLONY.

In the general remarks, included in his report on Public Works, for 1903 the Director of Public Works writes:—"The Naval Yard Extension and Messrs. Butterfield and Swire's shipyard works were still in progress throughout the year. In the case of the former, the sea wall and inner-wall or quay forming the pier on the north side of the Basin, a portion of the east wall of the Basin and the Murray Pier Extension and roadhead beyond it were all brought above low-water level, whilst the cofferdam enclosing the site of the graving dock was completed and the water pumped out."

In the case of the Quarry Bay Shipyard Works, the side walls of the graving dock for more than half its length were in progress and the excavation for the remaining portions well advanced, whilst the cofferdam to enclose the site of the entrance was nearing completion. Substantial progress was also made with the levelling operations, large masses of rock having been excavated and tipped to form the reclamation.

The large blocks of buildings on the Praya Reclamation between Ice House and Pedder Streets were in progress throughout the year, the triangular block at the junction of Des Vaux and Chater Roads, designated "Alexandra Buildings," being nearly completed. The south-eastern section of Princes Buildings was roofed in before the close of the year. The buildings for the Electric Tramway Power Station, on the east side of Bowington Canal, were nearly completed, and a new Chinese theatre was erected near the Gas Works, West Point. Substantial progress was made with the reclamation at Tsim Sha Tsui between Blackhead's Point and Kowloon Marine Lot 6.

## JAPANESE CHILDREN.

Japanese child must be quite delightful, judging from the description given of them by a French writer, who tells us some wonderful facts about the babies of Japan. He declares that "they never cry, and never make a noise," and that Japanese children "play without quarrels and without tears." A grave, self-possessed politeness is innate with these little people, and they behave with the most irreproachable *sans-froid* in the presence of their elders. The love of children is a veritable cult in Japan, and the little ones are surrounded with a care and solicitude that are almost religious. A story is told of a burglar who had broken into a house, and postponed his nefarious purpose to amuse a baby who was playing by itself and become so absorbed in his game that only at the entrance of the parents did he beat a retreat. Japanese children, we are told, are very fond of fairy tales, and several of our own familiar figures appear in their favourite stories, such as "The Ogre and Tom Thumb" and "Mother Goose." Many of these tales treat of animal, and among their records of talking beasts there is a dialogue between rabbits and crocodiles. The Japanese "recounters" show a kindly feeling towards animals and an intense love of nature. Thus it is that children in Japan will display a keen interest in natural objects especially in flowers and insects, of which last they are invariably tender in their treatment.

Yet the question of "child labour" has assumed a phase in Japan that is fortunately rarely reached in Western countries, and heavy work of all kinds is, strange to say, exacted from children to an extent which would surprise our humanitarian ideas. Child-actors and dancers, often not more than six years old, are habitually employed in the theatres, and fill the roles that we should allot to grown-up performers, with a finish and intelligence that are amazing. The little girl-dancers go through an exceptionally severe training in gymnastics to fit them for their profession. In the factories child labour is exploited to a considerable extent, children being employed for long hours at a stretch at a very low wage, while in public offices they are often engaged as porters and lacqueys at an absurdly early age, although they fulfil these functions with the utmost skill and propriety. Small wonder is it that the Japanese children are grave with a gravity far beyond their years, and take life seriously, for its burden falls early enough upon their small shoulders.

## CORRESPONDENCE.

(We do not necessarily endorse the opinions expressed by Correspondents in this column.)

## THE ATHLETIC SPORTS.

TO THE EDITOR OF THE "HONGKONG TELEGRAPH." SIR,—The Committee of the V.R.C., under whose auspices the athletic sports are to be held on the Happy Valley on Monday, are surely not to be congratulated on their liberality to the men in the Fleet. On every ship on the station a list of events for the forthcoming meeting is posted, presumably with the object of notifying intending entrants of the events open to them. To the surprise of the men in the Navy all the events, with only a single exception, are reserved to members of certain specified social clubs and the V.R.C. in the Colony. The only event for which the Bluejackets are allowed to compete is a Half-mile Flat race.

Surely if the competitions are got up in any sportsmanlike spirit the sailors in H.M.'s navy should not be excluded. There are many good men among them who can run and jump and by whose admission the various events on the programme will be more keenly contested and the flat, hurdle, and bicycle races will furnish more keen competition and greater interest: aroused amongst the spectators than the tame events of past meetings were capable of exciting.

I am one, among many, who hope to see that the hint will be taken by those responsible for the getting up of these annual sports and a larger number of events, in future, thrown open to men of the Fleet.—I am, etc.,

A BLUEJACKET.

Hongkong, 2nd April, 1904.

## SHORTHAND 2,000 YEARS AGO.

It will, no doubt, come as a surprise to most people that shorthand was known and practised two thousand years ago. Manilius, a contemporary of Caesar and Cicero, Virgil and Horace, asserts that some system of reporting very similar to our shorthand was in vogue in his days. Writing these words under the influence of Virgo and Mercury, he says they are:—

In shorthand skilled, where little marks comprise.

Whole words—a sentence in a single letter lies; And while the willing hand his aid affords, Prevents the tongue to fix the falling words. It is certainly a novel conception that Cicero's grand orations were committed to paper with as much skill as our modern stenographers boast.

## THE REAL TSAR.

## DEPRESSING PEN-PICTURE.

An anonymous writer in the *March Fortnightly* gives a depressing character-sketch of Nicholas II. His estimate is as follows:—

The Tsar Nicholas II is physically weak; his nerves are shattered; his will feeble. He is amiable where iron-handedness is essential. He is dependent when a strong individuality is required. He is constantly in subjection first to one influence and then to another. He is more than usually amenable to women's control; and the fair sex has on more than one occasion exercised powerful, if not calamitous, influence upon his life.

Naturally, the history of the Imperial house is suggested as one of the causes which have operated in the formation of the present ruler's characteristics. The details are too well known to be referred to here, and, in spite of them, as it were, the writer asserts that the Emperor Nicholas "is as right-meaning, benevolent, and clement a ruler as though he did not bear upon his shoulders accumulated memories of bloodshed, malignity, and strife." The record, however, goes to explain a constitutional melancholy to which he is subject, and, furthermore, as though to emphasize his gloomy retrospect, and to continue it in his own lifetime, the disaster on the Khodynsk plain during his coronation festivities is recalled.

A brighter view of the monarch as a dignified personality is presented in the following story of his meeting with the Kaiser in the Baltic:—"The Kaiser remonstrated an offensive message to the Tsar from the upper bridge of his magnificent yacht the *Hohenlohe*. It was couched in the following terms:—"The Admiral of the Atlantic salutes the Admiral of the Pacific." The Tsar was placed in a difficult position. If he accepted the implication on the British Foreign Office might have something to say. If, on the other hand, the signal remained unanswered, discourtesy to the Kaiser was the result. I am informed that the Tsar's reply was his own idea. He simply signalled—"Farewell." The laconic refusal to the German Emperor went round the floor of the world, and the "Admiral of the Atlantic" will not forget the quiet snub administered to him one grey afternoon in the shallow waters of the Baltic Sea.

The Tsar dreads war, and the writer of the article attributes this feeling to his mother. The sentiment has not tended to increase his popularity, and ever since the attempt upon his life by a Japanese fanatic at O su in 1891—and the unguarded statement by the Crown Prince of Greece, "Then Nickle ran"—there has been a doubt of his personal courage, which the writer suggests is cruelly unjust. He takes no delight in outdoor sports except in cycling, and his "amusements," which are few, include photography and the occasional production of melancholy verse, but he is a hard worker in affairs of state.

The article gives the following painful description of the young ruler's general surroundings:—"The atmosphere of suspicion and gloom produced by the constant sense of impalpable danger overshadows the spirits of everyone at the Russian Court. . . . The Tsar never knows who are his friends and who his enemies. An abiding sense of distrust is the result, and hence the passionate affection that his Majesty feels for his wife, whose faithful counsel and patient sympathy are the bright spots in one of the saddest lives in Europe. There is considerable pathos in the terse summing-up by the author of the article:—"The Tsar is a kindly, saddened, overworked, and unhappy man."

## TELEGRAMS.

## "HONGKONG TELEGRAPH" SERVICE.

## THE WAR.

## SINKING OF A PRESS BOAT

## BY THE RUSSIANS.

## CREW CAPTURED. THREE ESCAPED.

(From Our Own Correspondent.)

WEIHAIWEI, 31st March, 8.16 p.m.

When near the Miaotao Light-house, on the 26th inst., the Russians fired upon and sunk the s.s. *Hanyer*, chartered by the Osaka *Asahi* to serve as the despatch boat for that paper.

The crew were captured, with the exception of three who made good their escape.

## THE "MANDJOUR" QUESTION

## AT SHANGHAI.

(From Our Correspondent.)

SHANGHAI, 31st March, 6.20 p.m.

The *Mandjour* question is now settled, the dismantling of the vessel having been completed to the satisfaction of the Japanese Government. The Japanese cruiser *Akizushima* left this morning.

In our issue of the 29th ult., we stated that, according to a Peking despatch, the Japanese Minister, Mr. Uchida, had informed the Wai-wu that his Government was not satisfied at the way the dismantling of the *Mandjour* in Shanghai had been conducted. In other words, it was complained that only the breech-blocks of the big guns and the ammunition had been taken out of the vessel, while other most important parts of the machinery, etc., on board were not touched. Under the circumstances therefore the Japanese Government did not consider that the *Mandjour* had been actually made harmless.

\*The above telegrams were issued in an "extra" yesterday forenoon.—Ed., H.K.T.]

(Rutler's.)

## Parliamentary.

LONDON, 30th March.

Sir Michael Hicks-Beach has informed his constituents that he does not intend to seek re-election to Parliament.

The House of Commons will adjourn on the 14th proximo.

Mr. Balfour will probably introduce the Budget on the 19th proximo.

LATER.

The War.  
The Destruction of the Russian Quarantine Station at San-Shantao.

Russia having protested, through the French Minister at Tokio, against the destruction of the quarantine station at San-Shantao during the fourth attack on Port Arthur, as a violation of the Hague Convention, as previously telegraphed, Baron Komura, replying, said he was not aware of the destruction of the station, but the Hague Convention did not apply to Naval bombardments.

## The French Navy.

In a lengthy debate on the condition of the French navy, M. Lockroy sharply criticized the administration of M. Pelletan. The latter repeated the points of his defence on the 16th instant. M. Combes supported M. Pelletan. The Chamber eventually passed a resolution promising that a special inquiry into the state of the affairs of the fleet will be instituted.

## AN ACCIDENT AT TAIKOO.

A regrettable accident occurred yesterday evening at 9.30 at the Taikoo Sugar Refinery. Three coolies, who were working at the stacks, supplying fuel to the boiler house, were suddenly buried under a slip of descending coal. A gang at once set to work to dig out the unfortunate, and medical aid was sent for. When the bodies were recovered, life was found to be extinct; the three men having been crushed and suffocated by the weight and density of the coal.

## SHIPPING AND MAILS.

## MAILS DUE.

French (*Annam*) 4th inst.  
Indian (*Tringee*) 4th inst.  
American (*Doric*) 5th inst.  
Canadian (*Albatross*) 7th inst.  
Canadian (*Empress of India*) 15th inst.  
American (*Siberia*) 17th inst.  
Canadian (*Tartar*) 22nd inst.

The C. & M. Co.'s s.s. *Zafra* left Manila on yesterday afternoon and is due here on Monday at daylight.  
The Silk ex C. P. R. Co.'s s.s. *Taiwan* left Vancouver B.C. for Hongkong via usual Port of Call, on Thursday the 31st March.  
The Imperial German Mail s.s. *Seydlitz* which left here on the 2nd inst., at noon, has arrived at Genoa, on Wednesday at 8 p.m.  
The C. P. R. Co.'s s.s. *Albatross* arrived Kobe at 3.30 p.m., on Wednesday the 30th March, and leaves again at 8.30 p.m. on Thursday, the 31st for Shanghai, where she is due to arrive at noon on 4th inst.



## THE WAR.

## JAPANESE DIET.

## IMPORTANT DECISION.

On the 29th ultimo, the Japanese Diet, by a unanimous vote, adopted the following motion:—Considering that, in her war with Russia, Japan is struggling in a just cause, the Imperial Diet accepting, with a few trifling modifications, all the propositions of the Government relative to the financial measures rendered necessary by the present conflict.

## THE "MANDJOUR".

The following is a note addressed to the Japanese Consul-General, Shanghai, by Yuan Taotai of this port, as published in the *Sin Wan Pao* (28th ult.): Referring to your letter, requesting that prompt measures be taken to remove the navigating machines, guns, and fire arms from on board the *Mandjour*, and an explanation why the settlement of the question has been delayed, I beg to state that ever since telegraphically instructed by the Wai Wu Pu to find a suitable place for the armaments with the assistance of the Commissioner of Customs, many letters dwelling on the subject in question have been exchanged, which are now on file. As a suitable place could not be found in the Customs House, and the native junks were not fit for the purpose, it was then resorted to the late Fu Chi of the Kiangnan arsenal, but it was sold long ago. Under such circumstances I finally wired the Minister of the South, requesting him to send the *Nanjing* down here, where, after examination, the armament will be stored and sealed by the Commissioner of Customs and a special man appointed by the Minister. Whether the *Mandjour* had been fully disarmed or not I did not know for certain until yesterday when, in reply to my urgent letter, I heard from the Commissioner that the Russian Consul-General here expressed his objections based on the telegram received from his Minister, which did not fully set forth the point in discussion, and that in case of emergencies such as storm the ship would be placed in a precarious condition when her navigating machines are removed. I and the Russian Consul-General sent two long telegrams to Peking yesterday, addressed to the respective authorities. Besides writing to the Commissioner again on the dismantling of the *Mandjour*, I beg of you to order the *Atsushima* to leave accordingly.—Trans.

The following telegrams are from the A. C. D. News of the 28th ult.:

## THE LAST ATTACK ON PORT ARTHUR.

Tokio, 26th March.  
It is concluded, in reference to the engagement at Port Arthur on the 22nd instant, that the steaming out of the harbour of nine Russian men-of-war may be taken as indicating the revival of martial spirit in the Russian squadron since the arrival of Admiral Makaroff, but the gallant Admiral did not dare to engage the Japanese seriously with vessels incompletely repaired.

Admiral Togo also had no desire to come to close quarters with the Russian squadron while the latter was under cover of the land batteries. An indecisive result was inevitable.

The bombardment from Pigeon Bay by the *Fuji* and *Yushima* is believed to have had the effect desired, and it is apparent on a general view that the Russian ships are not in a position to do more than defend Port Arthur.

It is believed that the Russian squadron which steamed out to the outer harbour consisted of the battleships *Pervizet* (12,000 tons), *Petrovsk* (11,000), *Pobieda* (12,000), *Poliv* (11,000), and *Sevastopol* (11,000), and the cruisers *Askold* (7,000 tons), *Bayan* (8,000), *Diana* (7,000), and *Novik* (1,000).

## FINDING THE FUNDS.

Tokio, 26th March.  
The House of Representatives to-day adopted completely the financial war programme.

Regarding the compensation to the merchants concerned in consequence of institution of the tobacco monopoly it was feared that a scandal would ensue, and it was circulated among the members that the position of Mr. Matsuda, the President of the House of Representatives, was threatened, but the trouble was tactfully averted.

The method of compensation was modified to a proportion of the gross proceeds for three years.

## THE OFFENDING "NIROKU."

Tokio, 26th March.  
Mr. Akuyama, the proprietor of the *Niroku Shinbun*, has voluntarily resigned his seat in the House, on account of the defamatory article in his paper referred to in my telegram of the 22nd inst.

## THE GENEROSITY OF THE EMPEROR OF KOREA.

Tokio, 27th March.  
The Emperor of Korea has contributed 100,000 yen to the Japanese Soldiers' Relief Society.

## A POSSIBLE SIDELIGHT.

General Dragomiroff, a noted military authority, was summoned to St. Petersburg to assist in the war deliberations on March 5th. He severely criticised the policy pursued in the Far East by Russia, and insisted on the evacuation of Port Arthur in order to avoid greater disasters. His advice is unwelcome, and has been energetically repudiated.

## ITEMS.

In spite of the severe bombardment Port Arthur is being subjected to, theatres and restaurants are open as usual with bands playing in the park.

Sir Robert Hart has required all Russians and Japanese in employ of the Imperial Maritime Customs to sign declarations of strict neutrality during the Russo-Japanese war.

It is said the Russians have landed 36 siege guns of different calibre for the defence of Newchwang and neighbourhood. Preparations to contest the landing of the Japanese go on apace, and heavy guns are being mounted at the mouth of the river. Five regiments under General Kondratovich have been told off specially to hold this point. It is true that Japanese men-of-war have been seen outside Newchwang.

## BUDDHIST PRIESTS FOR THE FRONT.

*Rikishi* Rio Un Mukoyama Sugawa, one of Japanese pupil-monks of the Ven. H. Sumangala, Chief High Priest of Adam's Peak and Principal of Widyodaya College, Colombo, left Colombo for Japan by the last German boat. He will at once proceed to the scene of operations in the Far East, where he will be engaged in ministering to the sick and wounded, and the dying soldiers of the Japanese armies. Mr. Sugawa will represent the Buddhist priesthood of Ceylon. He is the first priest to go to the scene of operations in the yellow garb of Ceylon *Rikishi*. There are three other ordained Japanese monks under the High Priest, who are all residing at present at the Malakanda Temple.

## A TERRIBLE FLIGHT OF LOCUSTS.

## EXTRAORDINARY DETAILS.

An amazing account, which all reads like a bit of wild romance, is published in the current number of the *Humbly Catholic Examiner* of an extraordinary flight of locusts which visited Sataru recently.

Somewhat similar accounts of this strange and unprecedented visitation are given by other correspondents from the surrounding districts. The one which we extract is signed by H. Lithos, and dated Sataru, February 23rd.

Last week there passed over Sataru, the headquarters of the district, two or three swarms of locusts; but they were only the precursors of the terrible flight which passed through this station on the 20th and 21st inst.

On the evening of Saturday at 4 o'clock the locusts made their appearance, going from east to west; and as the evening approached myriads and myriads of these red creatures began to settle down to rest for the night. No one, however, had the slightest notion of the magnitude of the swarm or the extent of the damage they would cause during the night. The first report on Sunday morning was that the roads were blocked up by branches of trees falling across them, that the tongues from the station were retained, and that traffic was at a standstill; and the cause of all this was the locusts.

## AN APPALLING SIGHT.

Every one regarded these reports as preposterous and laughed at these wild stories. At 7 a.m. the locusts started on their onward course of destruction, taking the same direction as on the previous evening. It was then that one realized the awful immensity of the flight. As the day advanced the stream swelled. The main body, thick enough to give a good shade, occupied about one mile in breadth the two wings a mile and a half each. The extent of the swarm breadthwise was about 4 miles. A perfect sheet of red locusts on the ground, a thick cloud of them passing overhead, covered by these destructive creatures, the whole place was one mass of red insects.

There were people young and old crowded in their fields to drive the locusts away by beating tin and making a hideous noise enough to frighten the devil, but they shouted and shouted until they could shout no more, and sat down exhausted gazing at the vastness, the immensity, the infinity of the flight. It was a sight truly appalling.

## TREES COLLAPSE WITH WEIGHT OF INSECTS.

During Sunday large trees—mango trees, wild fig-trees, pipuls, etc., varying from 10 to 20 ft. and even more in circumference—trees that had withstood successfully many a violent storm—gleaning under the weight of the locusts, yielded like rubber and came down with a crash. But what is most surprising, nay, almost incredible, is that babul trees, which resist the sharp axe of the woodfeller, and even cactus, were not proof against these creatures.

There is not a single babul tree in the course followed by the locusts but is a monument with its branches hanging down broken, bearing testimony to the frightful devastation caused by them.

I can assure such of the readers of the *Examiner* as may be disposed to doubt the veracity of the above account that I myself had considered it chimerical and could not believe my own eyes even when I had visited the place.

## NEARLY THREE MILLION TONS OF LOCUSTS.

At a most modest estimate the locusts were flying at the rate of about 7 miles an hour, and they took 8 hours in passing they must have covered about 56 miles. Where the swarm was at its densest the locusts were not less than 20 to 30 deep; i.e. 20 to 30 layers of them; but taking them on an average and allowing two square inches of space for each locust, they amount to a great deal more than a billion, that is to say, an area of 13 figures. It is impossible for any one to count these figures; for though it may seem easy to speak of them it is quite a different thing when we try to comprehend them. We shall, therefore, employ the clock to do the counting for us. The clock makes 60 ticks every minute and hence 86,400 a day. At this rate our clock would take more than 35,643 years to accomplish this task. Let us take another instance—this time a commercial one. The locusts were found to weigh on an average 6 to 7 grains. Their total weight would therefore be about 2 millions and 91 thousand tons. If, after compressing them well, we had to transport them by rail it would require 3,000 G. I. P. Railway goods trains of 50 waggons each, carrying a load of 14 tons in each truck.

Ten times or possibly a hundred times the numbers given above may be nearer the truth, but what I have here endeavoured to do is to convey some slight idea what this gigantic flight was like.

## A POINT OF ORDER.

At a recent meeting of the Legislative Council of Colombo, N.E. the Governor (Sir Henry Blake), referring to the first question on the Agenda, in the name of the Hon. the Mercantile Member, said he was afraid the whole of the question could not be permitted, as, according to the Standing Orders, in any question, no argument or opinion was to be offered. The purpose of a question was to obtain information and not to express opinion. Nor should a question contain arguments, inferences, implications, and epithets. He believed the hon. member would see that the latter part of his question came within that category, inasmuch as it offered an opinion.

Mr. Figg: I submit to Your Excellency's ruling, and I would ask that the first portion of the question be put.

## THE NEW JAPAN.

## THROUGH FRENCH EYES.

## A "YELLOW RENAISSANCE."

M. Feliouen Challaye's article, "The Europeanisation of Japan," in the new number of the *Revue de Paris*, is of exceptional interest at this moment. In the first place, the substance of the article belies the title. M. Challaye shows that the Japanese hate and despise what Europe is pleased to call her civilization; and that their borrowings from Europe have been effected against the grain, and for the express purpose of beating vulgarisation Europe with her own weapons, driving her influence in the East. Only her vanity, says M. Challaye, has led Europe to fancy that the Japanese regard Western civilization as superior to their own, and that, therefore, they are "Europeanising" themselves purposely. The Western diplomatist who described modern Japan as a "bad translation" of Europe merely shared the delusion and conceit of foreigners generally. European residents in Japan, says the author, know nothing of the country and its people, not knowing their places of business. M. Challaye has gone about among the Japanese towns and country places, lived with the people, conformed to their manner of life, and so learned to understand them. And for the Japanese M. Challaye has a great love and admiration.

To say the least, "the Japanese have retained more of old Japan than they have borrowed or wish to borrow from Europe." "The everyday life of the home is still unaffected by Europe." M. Challaye then writes a charming account of the Japanese home, with all its simplicity and fine taste—seldom lacking even in the poorest. These qualities the Japanese have not borrowed from Europe. Between the houses of the rich and those of the poor, there is a kind of resemblance—of equality—which is anything but European; and the reason is that taste is a universal quality among the Japanese. "In no country," says William Morris' golden rule, "having nothing about you but what you know to be useful and believe to be beautiful, so scrupulously followed as in Japan." Again, the Japanese have not adopted European costume. Nor do they mean to. State officials wear European dress merely because they have to be in constant communication with representatives of foreign States. In private, they exchange their frock coats and trousers for the graceful, easy attire of old Japan. "The Japanese consider our European costume inconvenient, laughable, and absurd." "Unlike Europeans and savages the Japanese wear no jewels." Then follow several pages on the three great virtues of the Japanese—cleanliness, politeness, and gaiety. As regards the first, Europe has much to learn from Japan. "The politeness of the Japanese—a sign of 'equality'—the politeness of this hierarchical East is in striking contrast with the aggressive rudeness of our democratic communities. Gentleness, cordiality are the Japanese rule. No scenes of violence. The readiness with which Europeans fly into a passion stupefies the Japanese, and appears to them to be a sign of innate coarseness. In his consideration for others a Japanese habitually refrains from making any show of his personal sorrows." I can only refer the reader to M. Challaye's pages on the quiet gaiety and the refinement of the Japanese people's ordinary recreations, of their intense appreciation of natural beauty, their love of country walks, and their association of popular festivities, "not with memories of bloody combats, as in Europe," but with the changes of the year.

## TAKING STOCK OF CHRISTIANITY.

A Japanese acquaintance of mine, in one of his humorous moods, described to me how a Japanese Commissioner was dispatched to Europe to take stock of its Christianity, with a view to deciding whether the religion, as well as the ironclads, of Europe should be imported to the Island Empire. The Commissioner was so appalled by the conflict between "Christian" practice and profession that when he returned home and presented himself to the Mikado in Council he replied to his questioners with a slow, sad shake of his head. Whereupon, said my Jap, "we let the matter drop quietly, and we have stuck to our old 'hinto' religion ever since." I am reminded of my Japanese friend's irony when I read M. Challaye's passages on Japanese morals and on the three Japanese religions—Shintoism (which I may describe as a sort of civilized and developed animism, wherein nature and self, the dead and the living are indissolubly connected), Buddhism, and Confucianism—the second of which, non-theological, non-metaphysical, is little more than a rule-of-thumb code of practical morality. Buddhism of a reformed order—a sort of Protestant Buddhism—is making great progress in Japan. An interesting fact this is if taken in connection with (alleged) lead assumed by the Chinese and Japanese Buddhist hierarchy in what is called the Pan-Mongolian movement. The Christian missionaries, writes M. Challaye, make no way in Japan—though they have perfect liberty to teach and preach.

## THE REVENGE OF THE EAST.

But as a matter of fact, you say Japan has borrowed from Europe, re-creating whole departments of State upon the European model. Yes, M. Challaye replies; when the Americans and the Europeans half a century ago went with their cannon to open the Japanese door, the rulers and people of Japan realised "the horrible necessity" of meeting brute force with brute force; "they saw that unless they made themselves strong in war, in trade, and in finance, their superiority in morals, art, and religion would not save them from becoming the prey of the foreigner. They saw that without military protection of European States. To preserve their own civilization they must be independent, and to be independent they must arm themselves and oppose themselves to the West with the weapons of the West. The Japanese have borrowed nothing from Europe except what has made her separate States strong and independent."

And so the Japanese have become a first-class Power, by sea and land. "Under the garb of Europe the Japanese have preserved their old endurance, spirit of discipline and self-sacrifice, love of country, contempt for death. They are realising their national ambitions. They have succeeded in subjecting Europeans to the jurisdiction of the Native Courts. Europeans are not allowed to own land in Japan. . . . There are Japanese who cherish the dream of a yellow Renaissance under Japan's leadership—who hope that after planting herself firmly in Korea, Japan will come to China's aid, educating her innumerable population, initiating her into Western methods, showing her how to make herself invulnerable by Europe, then, in union with the new China, releasing the Asiatic races from exploiting, oppressive Europe, expelling the Americans from the Philippines, the French from Indo-China, the English from India; realising, in a word, the dream of a Far East for the Far Easterns, under the protection of the Empire of the Rising Sun."

## BRUTALISING EUROPEANISM.

Was there ever a dream so magnificent? Does M. Challaye believe in the possibility of its fulfilment? "If," he writes, "this dream should ever be realised, and Eastern Asia delivered from European supremacy, this would be the greatest blessing which the West herself could derive from her Europeanisation of Japan." But, however this may be, the deliverance would, in M. Challaye's estimation, be a great blessing to the Japanese themselves. In M. Challaye's view—and if I may borrow from Matthew Arnold an expression applied by him to another country and people—Japan has been barbarised, vulgarised, and brutalised by her importation of Western institutions. Japan has become a military power on the Christian model and Japan is now, to a great extent, as M. Challaye admits, brutalised by the very "militarism" which is the most offensively conspicuous and aggressive element in the life of some European States at this hour. "The West has been infecting Japan with the hateful spirit of Jingo-Imperialism. The new Japanese industrialism—European model—is demoralising great numbers of the Japanese population. 'The condition of the workmen—and especially of the workwomen—in the Japanese factories is frightful. . . . The contrast is terrible between the joyous life of old Japan and the sombre existence of the labourers of Europe.' The smoke of factories saddens, poisons the Japanese landscapes; 'the brutal regime of our industrial Europe has begun to crush the spirit of this happy Japanese race, to rob it of its joy of life.'"

## COMMERCIAL.

Shanghai advices, dated 28th ult., report business done:—Indo-Chinas at Tls. 64 for cash, at Tls. 64 for April and Tls. 66 July. Farnham, Boyds at Tls. 139½ and Tls. 140 for cash, Tls. 145 for June, Tls. 146½, Tls. 146½/145½ for July. Shanghai and Hongkong Wharves shares at Tls. 177½, Tls. 176½, Tls. 175 for cash, Tls. 170 for June and Tls. 172½/175 for July. Tugs at Tls. 30. Lands at Tls. 107. Langkats at Tls. 28½/28½ for cash. Tls. 295 for June and Tls. 297½/300 for July. Astors at \$34 Chinese Engineering and Mining shares at Tls. 6 "Beater."

## WEEKLY SHARE REPORT.

In their report dated 31st ult., Messrs. Benjamin, Kelly and Potts write:—

"To-morrow being Good Friday, we are printing our circular a day earlier than usual. During the week, the market has shown considerable activity and in most of our principal stocks, business has been done at enhanced rates."

The Fourteenth ordinary General Meeting of shareholders in the Yangtze Insurance Association, Limited, will be held at the head Office in Shanghai on the 21st April. The transfer books will be closed from the 11th to 21st proximo, both days inclusive.

Banks.—Hongkong and Shanghai Banks have considerably improved and after sales at \$610 and \$615 are now required for at \$317½. In London, the stock has risen to £627 Nationals are in request at \$ 51.

Marine Insurance.—Unions have again been dealt in at \$470 and are still in demand. China Traders are wanted at \$56. North Chinas remain unchanged at Tls. 67½ and Yangtses are quoted at \$135. Cantons continue weak at \$17½.

Fire Insurances.—Hongkong Fires have been booked at the advanced rate of \$285. China Fires have been, and can still be, placed at \$33.

Shipping.—Hongkong, Canton and Macao Steamboats have risen to \$ 6½, and are wanted at the price. In Indo-Chinas, an extensive business has been transacted at improving rates up to \$2; and the market closes very strong at this figure. The Shanghai quotation is Tls. 66 buyers. China and Mani's are slightly easier and can be obtained at \$32.

Douglas Steamships, have inquiries at \$34 Star Ferries have been disposed of at \$31 (old) and \$18½ (new), closing with further sellers.

Shell Transports are quiet at 19½. We quote Shanghai Tugs at Tls. 47½ for the 'ordinary' and Tls. 46½ for the 'preference' shares ex dividend of Tls. 2½ and Tls. 1½ respectively paid in Shanghai on the 29th instant.

Refineries.—China Sugars have strengthened their position, and are in request at \$113, after sales at \$110, \$111 and \$112. Perak Sugars have been at Tls. 50.

Mining.—There is no business to record in stocks under this head.

Docks, Wharves and Godowns.—Hongkong and Whampoa Docks are steadily with further sales at \$205. Since the settlement, Farrahams have materially improved, and are in demand at Tls. 145. Kowloon Wharves are firm at \$93 Hongkong Wharves have tumbled to Tls. 170, and shares are reported sold in Shanghai at this price.

Lands, Hotels and Buildings.—Hongkong Lands are quiet at \$145. Kowloon Lands can be procured at \$35, and West Points are out of favour at \$53. Hongkong Hotels have changed hands at \$135. Astor House Hotels have been sold, and have further inquiries at \$34. Humphreys Estates are unaltered at \$108.

Cotton Mills.—Quotations remain the same as last reported.

Cigar Companies.—Sumatra keep firm with buyers at Tls. 53.

Miscellaneous.—Green Island Cements have been disposed of at \$15. China-Bonuses have changed ownership and are in further request at \$8.10. A. S. Watsons are wanted at \$4, and A. S. Watsons can be placed at \$6½. The dividend of \$1 paid on the 29th instant Electric (old issue) have been fixed at \$13; the new shares have found investors, and there are further inquiries at \$13. Stearns Waterworks are to be had at \$153. Central Stores have been done at \$20. Langkats remain unchanged at Tls. 8.

## TO-DAY'S EXCHANGE.

London—Bank T.T. . . . . . 1/4  
Do. demand . . . . . 1/9 5/16  
Do. 4 months' sight . . . . . 1/9 9/16  
France—Bank T.T. . . . . . 1/22 1/2  
America—Bank T.T. . . . . . 1/22 1/2  
Germany—Bank T.T. . . . . . 1/22 1/2  
India T.T. . . . . . 1/22 1/2  
Do. demand . . . . . 1/22 1/2  
Shanghai—Bank T.T. . . . . . 1/22 1/2  
Japan—Bank T.T. . . . . . 1/22 1/2  
Singapore—Bank T.T. . . . . . 1/22 1/2  
Java—Bank T.T. . . . . . 1/22 1/2

4 months' sight Lf. . . . . 1/9 11/16  
6 months' sight Lf. . . . . 1/9 13/16  
30 days' sight San Francisco & New York 4 1/2  
30 days' sight Sydney and Melbourne 1/9 15/16  
4 months' sight France . . . . . 1/22 1/2  
6 months' sight " . . . . . 1/22 1/2  
4 months' sight Germany . . . . . 1/22 1/2  
1/2 Silver . . . . . 1/22 1/2  
Bank of England rate . . . . . 2 1/2

## OPUM QUOTATIONS.

To-day's quotations are as follows:—  
Malwa New . . . . . 940/970  
" Old . . . . . 980/1,060  
" Oldest . . . . . 1,100/1,160  
Patna New . . . . . 1,325  
Benares New . . . . . 1,315  
Persian (Paper) . . . . . 850/950

## MEN WANTED.

"It has been truly said that the great want of the age is men. Men of thought; men of action. Men who are not for sale. Men who are honest to the heart's core. Men who will condemn wrong in friend or foe; in themselves as well as in others. Men whose consciences are as steady as the needle to the pole. Men who will stand for right if the heavens totter and the earth reels. Men who can tell the truth and look the world and the devil right in the eye. Men who neither swagger nor flinch. Men who can take a note rapidly. Men who can type a good letter. Men who can keep true and faithful accounts. Men who are quick of figures. Men who can speak without vulgarisms. Men with good memories. Men with good manners, whether at business or in the social circle. Men with a knowledge of another language. Men who can have courage without whistling for it, and joy without shouting to bring it. Men through whom the current of everlasting life runs still, and deep, and strong. Men too large for certain limits, and too strong for sectarian bands. Men who know their duty and do it. Men who know their place and fill it. Men who mind their own business. Men who will not lie. Men who are not too lazy to work nor too proud to be poor. When in Office, the Workshop, in the Counting House, in the Bank, in every place of trust and responsibility, we can have such men as these, we shall have a Christian civilization—the highest and best the world ever saw."

Success in life. V. failure. The training given at the Business College, is the solution. Every boy and girl leaving School should have a few months at the Studio, Watkins Buildings, Queen's Road Central, (near G.P.O.) Hongkong. Warwick Peale, Principal. Circulars, post free.  
Hongkong, 2nd April, 1904. [465]

## GOVERNMENT BILLS.

TENDERS FOR SPECIE, BRITISH and MEXICAN DOLLARS, current in this Colony in Exchange for Sterling Bills drawn at 10 days' sight on the Lords Commissioners of His Majesty's Treasury, London, will be received by the Chief Paymaster, Army Pay Department, until 11 A.M., on the 6th April, 1904.

The Tenders to state the total amount (in Pounds Sterling), and the amount for which each Bill should be drawn, but no Bills will be issued for less than £100.

The Tenders to be in Duplicate, and in sealed covers, addressed to the Chief Paymaster, Army Pay Department, and endorsed "Tenders for Government Bills."

The right to accept or reject any or all of the Tenders is reserved.

Copies of Forms of Tender can be had on application.

GEO. H. FERRIER,  
Colonel, A. F. D.,  
H. M. Treasury Chest Officer.

His Majesty's Treasury Office,  
Fletcher Street,  
Hongkong, 31st March, 1904. [467]

## TO LET.

MEIRION, Nos. 1 and 2,  
BOWRING VILLAS, No. 2.  
Apply—  
HUGHES & HOUGH,  
8, Des Voeux Road,  
Hongkong, 2nd April, 1904. [463]

## To-day's Advertisements.

THIS ADVT. will appear 3 times ONLY, viz.: this issue, next WEDNESDAY, and finally on SATURDAY, the 9th inst. It will be wise to CUT OUT NOW, and Pin, Nail, or Paste Up. It spells "USEFUL."

## BUSINESS TRAINING COLLEGE.

TERMS:—  
SHORTHAND "Up-to-Date" System.—  
For full course to completion . . . \$ 50  
Or \$10 for the 1st lesson, \$10 for 2nd, \$10 for 3rd, \$10 for 4th, and \$1 each to completion of 21 lessons.

TYPE-WRITING (Blind Touch).—For the full course and use of machines, up to 6 months tuition and practice, including tabular work, balance sheets, &c. . . . . 100  
Or for 3 months' tuition and use of a machine, teaching correct fingering and the writing of a fair letter . . . 50  
Or, for the full course, on pupil's own machine, if purchased from the studio. 50

BOOK-KEEPING.—Daily lessons "Elementary" full course, one month . . . 25  
Daily lessons "Double Entry" full course, one month . . . . . 50  
Or for special trades, per hour . . . 3

DAILY LESSONS on "QUICK FIGURING."—New rules, adding backwards, new additions, amusing arithmetic, measuring for all trades, calculations for all branches of business per month . . . 50

LANGUAGES & TRANSLATIONS in English, Chinese, Japanese, Russian, Indian, Tongues, Malay, Spanish, Portuguese as used at Macao, French and German Daily Lessons, per month . . . . . 25  
Twice a Week Lessons, per month . . 10

DAILY LESSONS on FAULTS in SPEAKING and WRITING: Vulgarisms, misapplied words, Pleonasm, &c. For full course, one month. 50

DAILY LESSONS on MEMORY TRAINING: special methods adopted by many Professors; full course, one month . . . . . 50

DAILY LESSONS on MANNERS; BUSINESS and SOCIAL DEPORTMENT T.T. full course, one month . . . 50

The Pupils come and go at their leisure, during the hours of 8 o'clock a.m. and 9 o'clock p.m. daily, except—  
Saturdays from 8 to 2 o'clock noon.

Address: "THE SECRETARY,"  
BUSINESS TRAINING COLLEGE,  
Studio: Watkins Buildings,  
Queen's Road Central,  
HONGKONG.  
Near G.P.O. (P.O. Box 343).  
Hongkong, 2nd April, 1904. [466]

## ZETLAND LODGE.

No. 525, E.C.  
A REGULAR MEETING OF ZETLAND LODGE will be held at the FREEMASONS' HALL, Zetland Street, TONIGHT, the 2nd instant, at 8.30 for 9 P.M. precisely. Visiting Brethren are cordially invited to attend.  
Hongkong, 2nd April, 1904. [414]

## DOUGLAS STEAMSHIP COMPANY, LIMITED.

FOR SWATOW, AMOY AND FOCHOW.  
THE Company's Steamship

"HAICHING,"  
Captain Hodgins, will be despatched for the above Ports, on TUESDAY, the 5th instant, at 10 A.M.

For Freight or Passage, apply to  
DOUGLAS, LAURIE & CO.,  
General Managers.  
Hongkong, 2nd April, 1904. [464]

## Intimation.

THE POPULAR SCOTCH IS "BLACK & WHITE"

JAMES BUCHANAN & CO.,  
SCOTCH WHISKY DISTILLERS.  
By Appointment to  
H. M. THE KING  
and  
H. R. H. THE PRINCE OF WALES

Supplied at all the LEADING CLUBS and HOTELS, and to be obtained from LANE, CRAWFORD & CO, Queen's Road Central.

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## THE SHARE MARKET.

## TO-DAY'S QUOTATIONS

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div,  
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Also JEFFREY'S popular EDINBURGH XXX TOUT in cases of 7 doz. pints.



## NOTICE.

**THE WEATHER WILL BE EXCEEDINGLY COLD** AFTER YOU PASS PORT SAID ON YOUR WAY HOME AND COLDER STILL SHOULD YOU GO VIA U.S.A. OR CANADA. YET YOU CANNOT AFFORD TO LOSE THE SPLENDOR OF THE MEDITERRANEAN OR PACIFIC SEA BY NIGHT. THEREFORE A TRAVELLING RUG BECOMES A NECESSITY AND WM. POWELL, LTD., ARE PREPARED TO SUPPLY FINE SCOTCH RUGS FROM \$8.50 TO \$25.00. THESE WILL BE FOUND IN THEIR GENTLEMEN'S DEPARTMENT AT 28, QUEEN'S ROAD, WHERE THEY ALSO STOCK ALL OTHER TRAVELLING REQUISITES.

**BY THE WAY,** HAVE YOU HEARD THEY HAVE JUST RECEIVED 19 CASES OF NEW SPRING GOODS WHICH ARE NOW OPENED OUT, AND OTHER LARGER SHIPMENTS FOLLOW TO STOCK THEIR NEW PREMISES IN DES VCEUX ROAD WHICH WILL BE OPENED SHORTLY? ONE OF THE CHIEF FEATURES OF THE BUSINESS WILL BE THE GRAND WINDOW DISPLAY WHICH CANNOT FAIL TO ATTRACT THE ATTENTION OF EVERYONE IN THE COLONY.

**N.B.**—THE LADIES' DEPARTMENT IS AT PRESENT AT 34, QUEEN'S ROAD (UPSTAIRS).